

COMMITTEE REPORT

Planning Committee on
Item No
Case Number

13 January, 2016
05
15/4140

SITE INFORMATION

RECEIVED: 24 September, 2015

WARD: Barnhill

PLANNING AREA: Kingsbury & Kenton Consultative Forum

LOCATION: Lycee International De Londres, 54 Forty Lane, Wembley, HA9 9LY

PROPOSAL: Erection of a part basement building fronting Forty Lane to house a five lane swimming pool and studio with a green roof and associated works to include courtyard entrance, security gates, cycle parking, demolition and reinstatement of retaining wall, landscaping and installation of PV panels on the roof of the new Annex building

APPLICANT: Wembley Educational Charitable Trust

CONTACT: Nathaniel Lichfield & Partners

PLAN NO'S: Refer to Condition 2

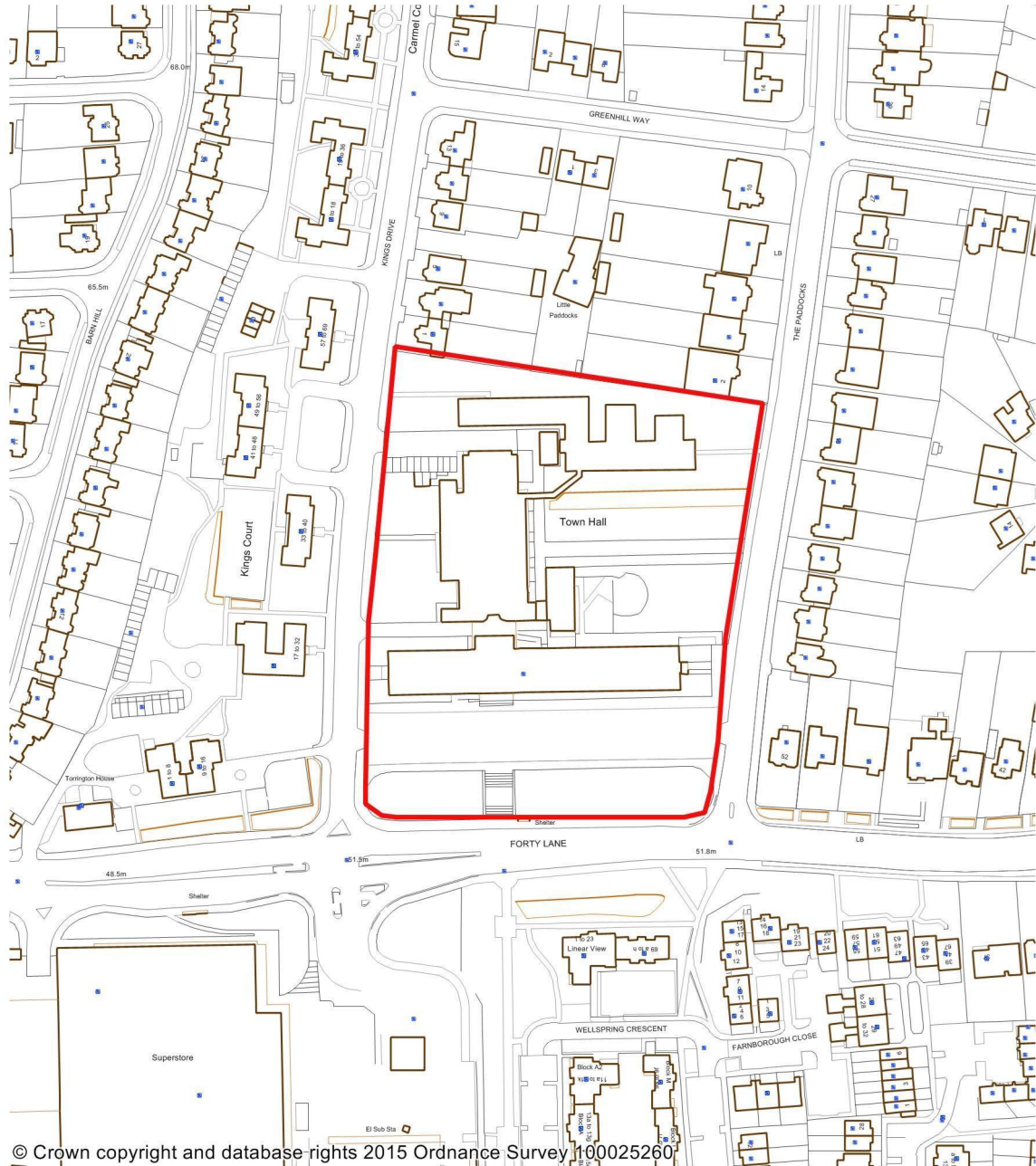
SITE MAP



Planning Committee Map

Site address: Lycee International De Londres, 54 Forty Lane, Wembley, HA9 9LY

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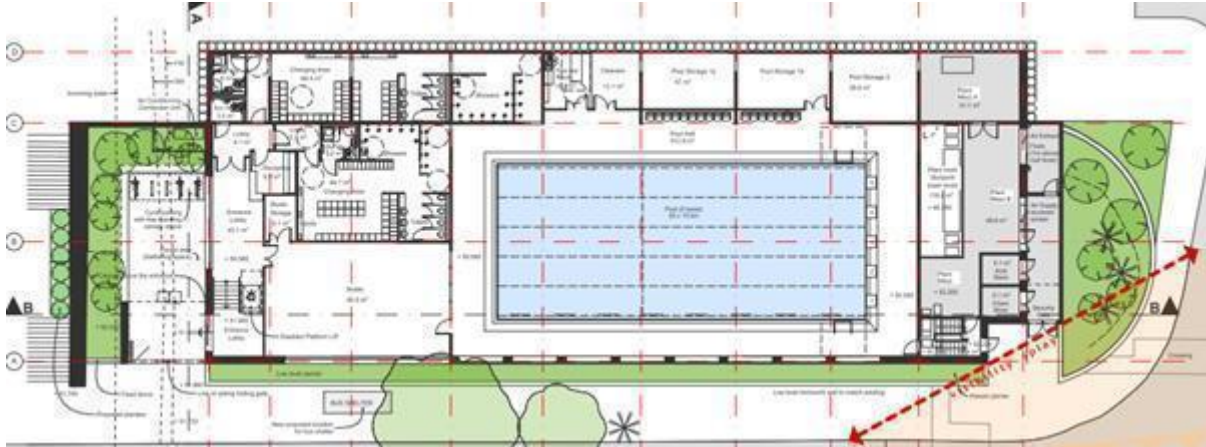


This map is indicative only.

SELECTED SITE PLANS

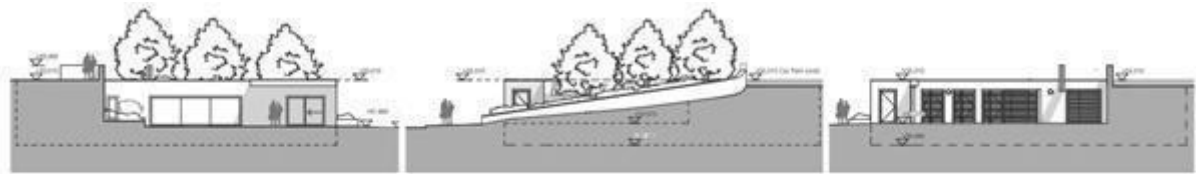
SELECTED SITE PLANS

Proposed Ground Floor Plan



The site plan illustrates the layout of the proposed parking lot. It features 44 numbered parking spaces (21-44) arranged in a row. A 'DROP OFF' zone is designated at the top of the lot, with arrows indicating the direction of traffic. A 'PICK UP' zone is marked at the bottom right with a red dashed arrow. The plan includes landscaping with trees and shrubs, a 'Curb' area, and a 'Drop-off/pick-up zone for bus vehicles'. The plan also shows a 'Curb' area and a 'Drop-off/pick-up zone for bus vehicles'.

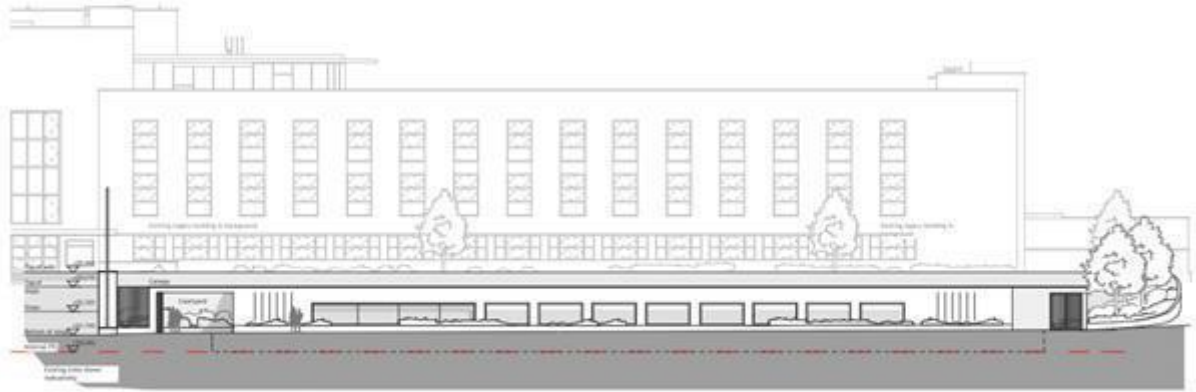
Proposed Elevations



1. West Elevation

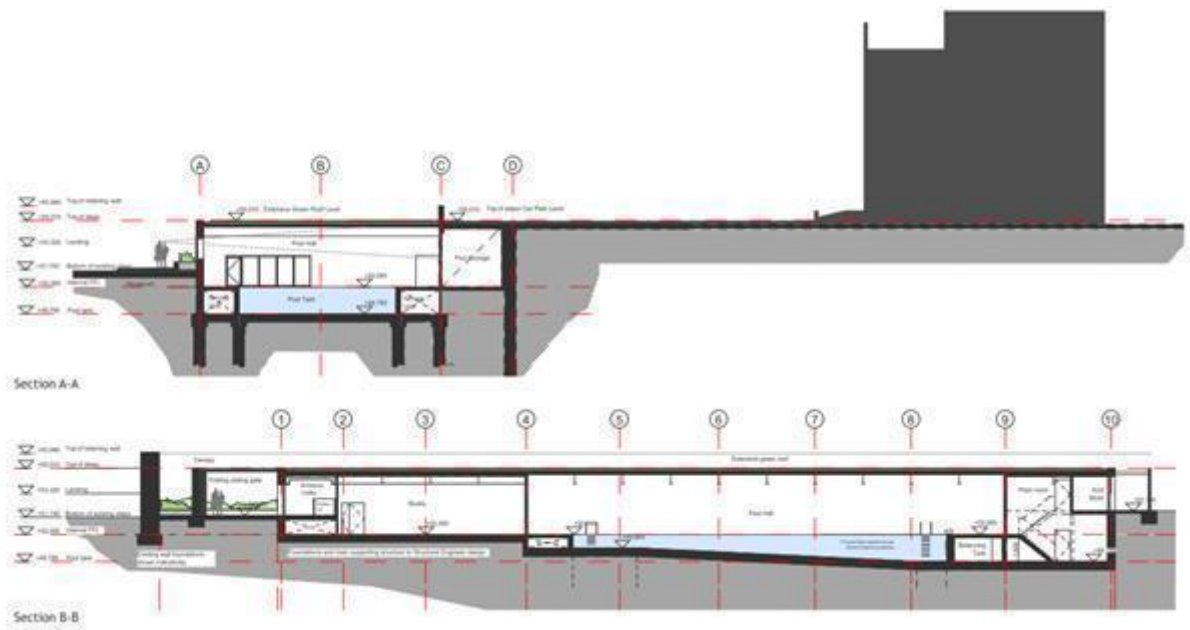
2. East Elevation

3. Plant Elevation (East)



4. South Elevation

Proposed Sections



Proposed Visual



INTRODUCTION

The application is reported to the Planning Committee under the provisions of Clause 24 of the Planning Code of Practice following the resolution at the previous meeting on 16 December 2015 of 'minded to refuse', contrary to the Officer recommendation to grant consent.

In making this resolution, Members raised concerns about the development related to reasons highlighted below. No additional material or information has been submitted by the applicant for Officers to comment on. Members are reminded that any appeals are assessed by the Planning Inspectorate and that appeal performance is a planning 'quality' indicator. Appeals also involve costs in terms of staff time and legal advice and the appellant's costs can be awarded if an Inspector considers that a reason for refusal to be unreasonable. The test of unreasonableness is different from not agreeing with the Council's decision.

(1) Concern about siting, height and design of the building and associated impact on the setting of and views to the Listed Building

As the report outlines, the proposal followed pre-application advice including with Heritage England and the 20th Century Society. However, the assessment of impact is a matter of judgement. If the Planning Committee are still minded to refuse on this ground then the following is a possible reason for refusal;

The proposed pool building, by reason of its design, size and siting and, in particular, its location within the principal frontage of the Grade II listed former Brent Town Hall, results in a detrimental impact on the setting of a Grade II listed building, reducing the visibility of the listed building when viewed from the immediate frontage of the property. This is contrary to Policy 7.8 of the London Plan (consolidated with further alterations since 2011) and the National Planning Policy Framework.

(2) Concern about the siting of the re-located bus stop and cage and potential impact on pedestrian and highway safety

Transportation have further advised that it is likely that the bus stop cage will not be required to be moved. However, if any relocation is required, this is likely to be limited to being some 2 to 4m eastwards, towards the traffic lights at the junction with The Paddocks. The submitted plans show some 40m from the existing bus cage to the traffic light stop line. If the cage was moved by up to 4m, this indicates that space would be retained for six cars to be able to stand in front of the bus cage when waiting at the traffic lights.

The submitted drawing indicates a new location for the bus shelter about 15m east of its current position. However, Transportation have advised that it would only have to move some 7m east to be clear of the building entrance. In the event that the bus stop cage is moved by up to 4m as described above, and the bus shelter being moved towards the start of the bus cage, a distance of some 4m will be maintained from the nearest point of the proposed entrance to the swimming pool building and the new bus shelter. On this basis, Transportation have reconfirmed their advice that the proposal is acceptable. The proposed relocation or removal of the BT kiosk does not raise any significant planning or listed building issues.

If, bearing in mind the discussion above, the Planning Committee are still minded to refuse consent then the following is a possible reason for refusal;

In the absence of specific details of the proposed relocation of the bus stop and shelter, the proposal is likely to result in conditions prejudicial to the free and safe flow of traffic on a distributor road in terms of the proximity of the bus cage to the traffic signals. This is contrary to saved policies TRN3 and TRN4 of the Brent Unitary Development Plan 2004.

Recommendation: Officers continue to recommend that permission is granted subject to the conditions set out in the original report and supplementary report.

The original report and supplementary report from previous Committee Agenda is set out below:

RECOMMENDATIONS

Grant Consent, subject to the conditions set out in the Draft Decision Notice.

A) PROPOSAL

Erection of a part basement building fronting Forty Lane to house a five lane swimming pool and studio with a green roof and associated works to include courtyard entrance, security gates, cycle parking, demolition and reinstatement of retaining wall, landscaping and installation of PV panels on the roof of the new Annex building

B) EXISTING

The application site comprises Lycee International de Londres Winston Churchill located on Forty Lane. It is a primary and secondary school with 9 forms of entry at secondary level for pupils aged 11 to 18 (total number of 900 pupils), and a single form entry primary school for pupils aged 5 to 11 (total number of 170 pupils). It will accommodate 1070 pupils when at full capacity.

The building is a Grade II listed building and formally accommodated the main civic offices of the former Borough of Wembley and subsequently Brent since 1940. The Town Hall was statutorily listed on 24 September 1990 at Grade II. The main civic offices of Brent are now located within the new Brent Civic Centre on Engineers Way.

The school occupies a site of approximately 2.1 hectares and fronts onto Forty Lane. As part of the change of use of the former Town Hall into a school, a new two to three storey annex was approved to provide additional school accommodation including a dining hall.

The main entrance to the building is accessed via Forty Lane with an access road and car parking running across the length of the building fronting Forty Lane at a higher level than the pavement on Forty Lane. There are significant level changes across the site with the land rising by around 9.4m from the pavement on Forty Lane to the base of the boundary wall to the north of the site.

The eastern boundary of the Town Hall abuts The Paddocks and its western boundary abuts Kings Drive. Both The Paddocks and Kings Drive are predominantly residential in character. Its northern boundary abuts the rear gardens of properties on Kings Drive and The Paddocks. To the south on Forty Lane is Asda Superstore, a Kwik Fit car garage and public house, with the Chalkhill Estate behind these buildings. Opposite the site on Bridge Road is the Ark Academy Primary and Secondary School and to the west beyond Kings Drive is the Barn Hill Conservation Area.

C) AMENDMENTS SINCE SUBMISSION

During the course of the application, the following amendments have been made to the proposal:

- Alterations to the corner of the swimming pool building on the Forty Lane/The Paddocks junction to not interfere with visibility splays
- Car parking spaces reduced to 46 to 44 spaces with increased tree planting in the car park

D) SUMMARY OF KEY ISSUES

Land Use and Nature of Application: This application seeks full planning permission for erection of a part basement building fronting Forty Lane to house a five lane swimming pool and studio with a green roof and associated works to include courtyard entrance, security gates, cycle parking, demolition and reinstatement of retaining wall, landscaping and installation of PV panels on the roof of the new Annex building. This is an existing school site and the principle is considered acceptable.

Impact on heritage asset (Listed Building): The proposed works do not cause substantial harm to the significance of the heritage asset. The works are to less significant elements within the curtilage of the listed building, most of which will be retained. The setting has been carefully considered and the views to the building will not be harmed. The new build has been carefully considered to reflect and complement the special interest of the listed building.

The new building, as a result of its siting and layout within the school site in compliance with SPG17 guidance, will not adversely impact on the amenity of neighbouring occupiers.

Landscaping and trees: A number of trees within the lower landscaped terrace where the swimming pool is to be located are to be removed. These will be replaced elsewhere within the school site in the car park or along the frontage with The Paddocks. The memorial trees will be replanted with new trees in Chalkhill Park and two memorial leaves provided on the memorial tree in the Civic Centre.

Transportation matters: The application will result in the loss of two car parking spaces from 46 to 44, as a result of the new tree planting. This still significant exceeds the maximum parking standards for a D1 use. Coach parking within the site will be available for other schools during school hours, and at weekends the school car park will be open for community users of the swimming pool. The bus shelter on Forty Lane will

have to be relocated in order to provide pedestrian access.

E) MONITORING

The table(s) below indicate the existing and proposed uses at the site and their respective floorspace and a breakdown of any dwellings proposed at the site.

Floorspace Breakdown

Primary Use	Existing	Retained	Lost	New	Net Gain (sqm)
Non-residential institutions	12580	12580	0	1194	13774

Monitoring Residential Breakdown

Description	1Bed	2Bed	3Bed	4Bed	5Bed	6Bed	7Bed	8Bed	Unk	Total
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RELEVANT SITE HISTORY

Relevant planning history

Listed building application accompanying this full application:

15/4141: Listed Building Consent sought for erection of a part basement building fronting Forty Lane to house a five lane swimming pool and studio with a green roof and associated works to include courtyard entrance, security gates, cycle parking, demolition and reinstatement of retaining wall, landscaping and installation of PV panels on the roof of the new Annex building - under consideration.

Previous consents relating to the use of the site as a school and associated works

13/1995: Full Planning Permission sought for Change of use and refurbishment of the existing Town Hall (Sui Generis) into a new primary and secondary French International School (Use class D1) involving the demolition of a number of ancillary buildings (single storey pre fabricated building and freestanding garages to the rear, and the existing print room attached to the main Town Hall building) and the erection of a part 2/part 3 storey extension along with associated works, subject to a Deed of Agreement dated 28 January 2014 under Section 106 of the Town and Country Planning Act 1990, as amended - **Granted, 29/01/2014.**

13/1996: Listed building consent for the change of use and refurbishment of the existing Town Hall (Sui Generis) into a new primary and secondary French International School (Use class D1) involving the demolition of a number of ancillary buildings (single storey pre fabricated building and freestanding garages to the rear, and the existing print room attached to the main Town Hall building) and the erection of a part 2/part 3 storey extension along with associated works - **Granted, 29/01/2014.**

CONSULTATIONS

A joint consultation letter was sent out to 268 neighbouring properties for the full planning application and listed building consent application on 9 October 2015.

Site Notice: 27/10/2015 - 17/11/2015

Press Notice: 15/10/2015 - 05/11/2015

Public Consultation

Two letter of support received on the following grounds:

- The opening of the school represented a significant and positive investment in the old town hall building.
- The site proposed for the swimming pool is currently under-utilised and has little environmental/ecological merit.
- The proposed design includes suitable environmental features to enhance the site.
- Approval by the Council will demonstrate the Council's continued commitment to the school and its development, with consequent benefits to the wider Wembley community.
- New infrastructure that will benefit everyone in the neighbourhood.

QARA

No objections to the proposal in principle. Has queried whether there is scope to provide more landscaping on the roof.

Statutory Consultation

Historic England

No objections raised. Advised that the application is determined in accordance with national and local policy guidance, and on the basis of Brent's conservation advice.

Greater London Archaeological Advisory Service (GLAAS)

No objections raised.

The Twentieth Century Society

No objections subject to conditions being imposed to secure details of the precise brick colour and size and details of signage that should be conservation signage.

Transport of London

No objections raised subject to the following:

- During the construction of the site the bus stop should not be blocked or impeded from its normal usage in any way.
- A Travel Plan and Construction Logistics Plan should be conditioned.

Internal Consultation

Heritage Officer

Advised that the proposal will sustain the significance of heritage assets. Have advised that conditions should be applied to the following:

- Full recording of the frontage for the London Parks and Garden's Trust Register
- A brickwork panel of the new brick bonding to be approved on site before the commencement of works.
- a brickwork panel using the old bricks to be approved on site before the commencement of works.
- Details of new signage.
- Colour and section of the window frames, associated boundary treatment, external doors and hard landscaping.
-

Landscape

No objections in principle. Replacement trees to be planted within the school site.

Concerns have been raised with the use of *Pyracantha* and *Berberis* in narrow planting areas fronting the street. Alternatively planting should be secured by condition.

Sustainability

No objections raised.

Transportation

No transportation objections subject to the following conditions

- (a) A revised drawing to be submitted showing the boundary wall, near the junction, to be no higher than 1m, in order to maintain junction visibility.
- (b) The bus stop relocation, proposed new shelter, removal of the existing shelter and relocation of the bus cage marking should be arranged with Transport for London and Brent Council as a Highway Authority.

- (c) The school car park is made available to community use of the pool
- (d) A revised travel plan should be submitted for LPA approval.

POLICY CONSIDERATIONS

The Town and Country Planning (Listed Buildings and Conservation Areas) Act 1990

The above legislation provides specific protection for buildings and areas of special architectural or historic interest. Section 66(1) of the Act states that in the consideration of proposals for planning permission which affect a listed building or its setting, the local planning authority shall pay special regard to the desirability of preserving i) that setting, or ii) any features of special; architectural or historic interest it possesses.

National level policy and guidance

The National Planning Policy Framework (NPPF) was published in March 2012.

Paragraph 72 of the NPPF attaches great importance to ensuring that a sufficient choice of school places is available to meet the needs of existing and new communities, and requires Local Planning Authorities to take a proactive, positive and collaborative approach to meeting this requirement, and to development that will widen choice in education.

Paragraph 126 of the NPPF outlines that the conservation of heritage assets can bring wider social, cultural, economic and environmental benefits. In accordance with paragraph 128, applicants are required to describe the significance of the heritage asset affected by a proposal, including any contribution made by their setting. Paragraphs 133 and 134 provides guidance with regards to the impact of a proposal on the significance of the heritage asset, and states that where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal, including securing its optimum viable use.

Regional level policy and guidance

The London Plan - The Spatial Development Strategy for London Consolidated with Alterations since 2011 (adopted March 2015)

Strategic planning in London is the shared responsibility of the Mayor of London, 32 London boroughs and the Corporation of the City of London. Under the legislation establishing the Greater London Authority (GLA), the Mayor has to produce a spatial development strategy (SDS) – which has become known as ‘the London Plan’ – and to keep it under review. Boroughs’ local development documents have to be ‘in general conformity with the London Plan, which is also legally part of the development plan that has to be taken into account when planning decisions are taken in any part of London unless there are planning reasons why it should not.

Key policies include:

- 3.18 Education Facilities
- 3.19 Sports Facilities
- 5.2 Minimising Carbon Dioxide Emissions
- 5.3 Sustainable Design and Construction
- 5.6 Decentralised Energy in Development Proposals
- 5.7 Renewable Energy
- 5.9 Overheating and Cooling
- 5.13 Sustainable Drainage
- 6.1 Strategic Approach
- 6.9 Cycling
- 6.10 Walking
- 6.13 Parking
- 7.4 Local Character
- 7.6 Architecture
- 7.8 Heritage Assets and Archaeology

Local level policy and guidance

Brent's Core Strategy 2010

Objective 5 - meeting social infrastructure needs

CP7: Wembley Growth Area

CP19: Brent Strategic Climate Change Mitigation and Adaption Measures

CP23: Protection of existing and provision of new Community and Cultural Facilities

Brent Unitary Development Plan 2004

Within the 2004 UDP the following list of saved policies are considered to be the most pertinent to the application.

BE4: Access for Disabled People

BE6: Public Realm - Landscape Design

BE7: Public Realm - Streetscape

BE9: Architectural Quality

BE22 Protection of Statutory Listed Buildings

BE23 Setting of Listed Buildings

BE30 Enabling Development & Heritage Asset Conservation

BE31 Sites of Archaeological Interest

BE12: Sustainable Design Principles

BE17: Building Services Equipment

EP2: Noise & Vibration

EP3: Local Air Quality Management

CF8: School Extensions

TRN4: Measures to make Transport Impact Acceptable

TRN10: Walkable Environments

TRN11: The London Cycle Network

TRN22: Parking Standards - Non Residential Developments

PS12 - Non-Residential Institutions

DETAILED CONSIDERATIONS

Introduction

1 Lycee International De Londres Winston Churchill is a Grade II listed building. Up to 2013, it had accommodated the main civic offices of the former Borough of Wembley and subsequent Brent since 1940. The building was designed by the architect Clifford Strange in a Scandinavian influenced Art Deco moderne style.

2.. This application seeks planning permission for a five lane indoor swimming pool and studio space. The building will be located within the lower landscaped area to the front of the site facing Forty Lane. It will be located to the east of the main stepped entrance to the building from Forty Lane.

3. The development will include the following works:

- Erection of a part basement building to accommodate a five lane swimming pool and studio with associated storage, plant, showers and changing areas;
- Creation of a courtyard entrance to the west of the new building with security gates
- New landscaping including replacement tree planting and installation of green roof on the swimming pool structure
- Demolition and reinstatement of the existing retaining wall and increase in height by five brick courses
- Installation of PV panels on the roof of the new Annex Building
- Relocated of the bus shelter on Forty Lane and associated works to the bus cage

4. An application for listed building consent has also been applied for and will be considered parallel to this application. The listed building consent focuses on heritage matters, whereas the planning application will discuss wider issues associated with the proposal.

Principle of development

5. The principle of development of education uses on the site has been established through the granted of planning references: 13/1995 and 13/1996. The former Town Hall has now been converted into the Lycee which opened in September 2015. The principle of the development is supported in policy terms at all levels as set out in the policy context above.

6. The proposed swimming pool will enhance the Lycee's sports offer for its students. This is in accordance with London Plan Policy 3.18 which supports school proposals which enhance education and skills provision, including expansion of existing facilities.

Community Use

7. London Plan Policy 3.16 requires that, wherever possible, the multiple use of social infrastructure should be encouraged. Policy WEM37 of WAAP supports the Council working in partnership with schools to make new or upgraded sports facilities available for the local community use out of school hours.

8.. Community access is proposed for the new swimming pool. Details of which are set out in the Community Access Plan submitted with the application. This proposes the following community access:

- The swimming pool will be open during the week for use by local schools for four half day sessions
- Community use of the swimming pool and studio facilities is anticipated to be before school use on weekdays, between 7 and 9am, and after school, between 7 and 9.30pm
- Community access for swimming lessons is anticipated on two early evening sessions per week, as well as weekend mornings. General community access to the pool facilities is expected on weekend afternoons and during holiday periods. However, the opening times for the facilities during these periods are yet to be defined
- The swimming pool will be operated by an external management company

9. Further details of the community access arrangements will be set out in an updated Community Access Plan to be secured as a condition to any forthcoming planning consent. The Community Access Plan should establish the range of facilities which will be made available, at which times and hours, establishing the means by which the facilities can be booked including a contact within the school and ensuring rates of hire comparable to similar local authority facilities.

10. Whilst community access within the main school buildings of the Lycee were secured for a minimum of 15 hours per week as part of planning application reference 13/1995, given that the swimming pool facilities will be made available during weekdays and weekends, it is considered appropriate to secure a minimum of 30 hours per week. This is comparable with other school facilities in the locality that offer use community use of sporting facilities such as Ark Academy.

Design and Listed Building Status

11. The new swimming pool is to be sited at the front of the site within the lower landscaped terrace area. It is to be sited to the east of the main step entrance located on Forty Lane. The siting of the new swimming pool building on the eastern side of the existing steps, respects the asymmetric principal elevation of the building. The building is to be set back 1.4m from the Forty Lane frontage to allow the low boundary wall to be retained and rebuilt in section. To address highway concerns with visibility splay, the south eastern corner of the building has been stepped back.

12. The building will be a partial basement construction and when viewed from Forty Lane it will appear as a single storey building. It has been designed so that the roof of the swimming pool building is no higher than the car park level. The sinking of the building into the landscaped area will allow the front facade of the main building to remain visible, and for the extension to read as a subordinate addition rather than compete with the main building.

13. The building will accommodate a 5 lane 10.5m x 25m length swimming pool. A 95.5sqm studio is also proposed. There will be single sex changing facilities for 60 people; 30 male and 30 female. Additional disabled changing and showering facilities have been provided.

13. The new swimming pool building is to be in brick. The bricks will be the same proportions as the existing building, but will be a different colour to allow the new building to be recessive rather than dominant the existing building. The overall mass and scale sustains the appearance and setting of the former Town Hall.

14. The fenestration of the proposed swimming pool building has been designed to respect the main building. The form, spacing, size and rhythm of the windows are aligned with the corresponding windows of the main building. The windows closest to the entrance of the swimming pool building have been treated in a different manner, to be merged to form one continuous window, to differentiate and signal the entrance end of the proposed building. Details of the depth of window have reveals have been indicated, which suggests two possible options. One is a deeper window reveal at 0.28m and the other is flush window reveal at 0.03m. It is recommended that further details of the window reveals are conditions to ensure that this detail is simple in its form and does not compete with the main building.

15. It is recommended that full details of external materials are conditioned to any forthcoming consent, together with full details of signage.

16. The plant room is located next to The Paddocks. This area will partially be screened by the existing retaining wall rising up along The Paddocks. A perforated brick wall is also proposed to restrict views of the louvers and doors to the plant equipment. A flue is proposed that is no higher than the retaining wall to the car park. It is recommended that further details of the flue are conditioned together with covering over the roof of the plant area.

Inclusive access

17. The proposed pool is designed to be fully compliant with Part M of the Building Regulations 2013 and BS 8300. Level access is maintained to the courtyard and the building from Forty Lane. As the swimming pool and changing rooms is at a lower level to street level, an internal platform lift is proposed. As discussed above disabled WCs and changing areas will be provided.

18. It is recommended that a condition is secured requiring details of an access strategy for the building to be provided to ensure inclusive access is provided for all users.

Impact on neighbouring occupiers

19. The location of the new building within the lower landscaped terrace fronting Forty Lane will be sufficiently set in from the boundaries with residential properties. As such it is not considered that the building will adversely impact on the amenities of neighbouring residential occupiers through loss of privacy, outlook or daylight/sunlight.

20. As plant equipment is proposed within the new building, officers in Environmental Health have requested for a condition to be secured to any forthcoming consent requiring plant and ancillary equipment to be 10dB(A) below the measured background noise level when measured at the nearest noise sensitive premises.

Landscaping and Ecology

21. The area where the swimming pool building is to be sited currently contains a grassed area with approximately 18 trees. A tree survey has been submitted identifying five moderate quality (category B) and 13 low quality (category C) trees to be removed. Two of the trees are memorial trees.

22. Fifteen new trees are proposed to be planted elsewhere within the site including four new trees in the car park and new tree planting along the frontage of the building with The Paddocks.

23. Officers in Landscape Team have confirmed that the loss of trees are acceptable. It is recommended that full details of the replacement trees are secured by condition. They have also raised concerns with the use of *Pyracantha* and *Berberis* in narrow planting areas fronting the street. This is because these are thorny bushes, *Pyracantha* especially being of vigorous growth habit. This species will either grow up to obscure swimming pool windows, or grow out to spread above the public footpath. They have recommended using smaller, less vigorous shrubs. Once again, this can be secured through condition.

24. In terms of the memorial trees, funding has been provided by the applicant to secure two trees to be provided within the memorial garden in the Civic Centre, and for two new trees to be planted in Chalkhill Park. This is to be secured via a Unilateral Undertaking under Section 106.

25. It is also recommended that full details of the green roof above the swimming pool are conditioned.

26. An updated Ecological Report has been submitted. This recommends the following measures are secured as part of the planning application:

- Planting of habitats within the green roof which is of value to wildlife
- Provision of nesting/roosting habitat such as nest boxes and bat boxes
- Retained trees on site to be protected during construction works
- Vegetation clearance to be undertaken outside of the nesting bird season
- Covering of excavations for terrestrial mammals including hedgehogs

27. The above measures will be conditioned as part of any forthcoming consent.

Flood Risk/Drainage

28. The application is accompanied by a Flood Risk Assessment. The site is located within Flood Zone 1, and therefore, is at low risk of flooding.

29. Drainage proposals for the site have also been included. To control the rate of run off, Sustainable Drainage Systems (SUDs) have been considered. A green roof is proposed together with an attenuation tank.

30. It is recommended that full details of surface water drainage and foul drainage are conditioned to any forthcoming consent.

Sustainability

31. Achieving sustainable development is essential to climate change mitigation and adaptation. The most recent relevant policy framework includes Brent's adopted Core Strategy 2010 policy CP19 Brent Strategic Climate Change Mitigation and Adaptation Measures and the London Plan policies within Chapter Five London's Response to Climate Change.

Compliance with Brent policies

32. In support of the objective of satisfying Core Strategy policy CP19 Brent Strategic Climate Change Mitigation and Adaptation Measures, a Sustainable Statement and Checklist has been submitted predicting the scheme will achieve BREEAM 'Excellent'. Policy CP19 seeks to achieve BREEAM 'Excellent' for new commercial buildings. It is recommended that BREEAM 'Excellent' is secured as a condition.

33. Brent's Sustainability Checklist has been completed, that demonstrates that the scheme can achieve a score of 44% This score falls marginally short of the target of 50%. Such a short fall is considered marginal and does not warrant a reason for refusal. It is recommended that a minimum score of 44% for the Sustainability Checklist is secured as a condition to any forthcoming consent.

Compliance with Further Alterations to the London Plan 2015

34. The scheme includes measures to minimise the impact of this proposal on, and mitigate for the effects of, climate change and your officers consider the proposal to be in accordance with the energy hierarchy as required by Further Alterations to the London Plan 2015 policy 5.2 Minimising carbon dioxide emissions part (a): (i) be lean: use less energy; (ii) be clean: supply energy efficiently; (iii) be green: use renewable energy.

35. In summary, the proposal exceeds the criteria of London Plan policy 5.2 for 35% improvement on Part L 2013 Building Regulations as a carbon saving of 36.8% is proposed.

- Lean measures

36. The building has been designed to maximise the Passive Energy design measures in order to reduce energy demand of the development. This has been achieved by increasing the thermal performance of the building fabric by reducing U-values and air permeability. This achieved 10.65% reduction beyond the TER.

- Clean measures

37. A combined Heat and Power system is proposed with back up boilers. This further improves the compliance with Part L to 31.7%.

38. It is noted that the scheme is not accompanied by an Air Quality Assessment or details regarding the emissions from the proposed boilers. It is therefore recommended that a condition is attached to this consent to secure these details.

- Green measures

39. PV panels are proposed. These are to be included on the roof of the annex building rather than the roof of the swimming pool building. PV panels are not considered appropriate on the swimming pool building as they are too low and would impact on the setting of the main building. The total carbon reduction including the PV panels is 36.8%.

40. The carbon reduction measures set out above are recommended to be conditioned to any forthcoming consent.

External Lighting

41. Details of external lighting for the swimming pool building has been provided. It is recommended that full details of the external lighting including spillage diagrams are conditioned to any forthcoming consent.

Transportation

Car parking

42. The car parking allowance for the school (Use Class D1) is given in standard PS12 of the UDP.

43. As a result of revision to the landscaping plan to provide replacement tree planting within the car park, the number of spaces within the car park has reduced from 46 spaces to 44 spaces. This still exceeds the maximum allowance of 23 spaces.

44. The Lycee proposes for the pool to be used by the school, as well as other schools in the area. They intend to have only coaches of children visiting from other schools for swimming galas etc. Although the car park does not have a dedicated coach parking space, there is a drop off area. Details of coach tracking with the vehicles entering from Kings Drive and exiting from The Paddocks have been provided confirming that this can safely occur within the existing layout. Therefore the existing off-street parking spaces do minimise any concern for overspill parking during school term.

45. The pool and the studio are also proposed to be used by the wider community outside of school hours in the evening and at weekends. The Paddocks and Kings Drive directly flanking the site can accommodate 26 on-street parking spaces and the streets are not defined as heavily parked during the evenings and weekends and therefore a lack of off-street parking is broadly acceptable in principle.

46. The community access plan proposes opening hours of 7-9am and 7-9.30pm on weekdays and at weekends, although they plan to only finalise opening hours once the pool is open and in use. The plan assumes a maximum of 20 people using the pool during the weekday evenings and on-street parking will be sufficient for this.

47. However, the plan also suggests 60 people per hour plus parents during weekends for swimming lessons. This is a large number of people expected for swimming lessons and it cannot be assumed that majority of them will not travel by car. In addition to this, the weekend swimming lessons could result in parents dropping off children near the junctions on Forty Lane. Your officers have therefore requested that the school's car park should be made available for weekend swimming lesson sessions in order to allow safe parking and set down/pick up area. This will be secured by condition as part of a car park management plan.

Visibility

48. The swimming pool building is proposed in the southeastern corner of the site, close to the junction with The Paddocks/Forty Lane. As this is a signalised junction, junction intervisibility zone requirements, as set out in TD50/04 of the Section 6 of the Design Manual for Roads and Bridges, apply to the junction. It is essential that this zone is not obstructed by any building in order to permit manoeuvres to be completed safely once the driver has entered the intervisibility zone.

49. The zone is defined as the area bounded by measurements from a distance of 2.5m behind the stop line

extending across the full carriageway width of each arm. The plans originally submitted with the application, showed the new building, particularly the chemical and plant area, interfering with the junction intervisibility between the stop line on Forty Lane (eastbound) and the associated exit lane on The Paddocks.

50. A revised drawing has been submitted. This now shows the chemical and plant areas set back, as well the security gate, to remove any interference within the junction intervisibility between the stop line on Forty Lane (eastbound) and the associated exit lane on The Paddocks. Officers in Transportation have confirmed that this revision is acceptable.

Access

51. The entrance to the swimming pool building will have a folding metal gate for the pedestrian access. This is acceptable as the gates do not open out onto the Public Footway, in compliance with Section 153 of the Highways Act 1980.

52. The security gate to the plant room has doors that open outwards on to the footway however, this appears to be on the private land and therefore acceptable. As this security gate is not located on the back edge of the public highway, it is recommended that demarcation is kept between the Public Footway and private forecourt and that studs are implemented to show this demarcation, to ensure that there is no confusion with regards to ownership and cleaning of the streets.

53. Full details of the design of the gates and the demarcation measures are recommended to be conditioned to any forthcoming consent.

Relocation of the bus stop

54. The bus shelter on Forty Lane will have to be relocated in order to provide pedestrian access. The applicant has been in discussions with TfL and Brent Council traffic engineers to agree an alternative location. The proposed location is just east of the new pedestrian access which will be closer to the stop line. Bus stop relocation, proposed new shelter, removal of the existing shelter and relocation of the bus cage marking should be arranged with Transport for London and Brent Council as a Highway Authority. These measures will be conditioned to any forthcoming consent.

Travel Plan

55. Planning permission granted 13/1995 does have a Section 106 Agreement incorporating the requirement of a Travel Plan. An interim school travel plan has been submitted with this application.

56. Table 2.3 and 2.4 proposes a predicted modal split of the primary and secondary school, based on Travel Plans carried out for the existing London French schools in Camden and South Kensington. The majority of pupils are expected to travel by public transport and walking to school. Only 20% are expected to travel by car.

57. Table 2.5 predicts a modal split for staff which proposes only 28% travelling by car with majority using the underground to commute.

58. Point 2.26 proposes a baseline modal split to be undertaken during the first term. The school has been opened since September 2015 and therefore a revised travel plan should be submitted with a completed 'hands up' survey.

59. The travel plan includes initiatives such as walk on Wednesdays, bike week etc. The school intends to liaise with TfL to attend the schools assembly to give talks on sustainable modes. The school should note that Brent Council's School Road Safety team do carry out assembly talks as well as encourage WoW, Road Safety visitors, cycle training and many other schemes.

60. After accessing the travel plan using the ATTrBuTE tool, it has failed for the following reasons: Targets should also be set out for 3-5 years after occupation and a baseline modal split should now be provided as the school has been open since September 2015. It is recommended that a revised Travel Plan addressing these matters are conditioned as part of any forthcoming consent.

Construction Management Plan

61. It is recommended that a Construction Management Plan and a Construction Logistics Plan are secured

through condition. This document will include details on how the bus stop and pavement are not blocked or impeded from its normal usage during construction, details of deliveries arrangement and construction workers parking. The document will also require a photographic dilapidation survey of the pavement before construction works commence, so that any damage to the pavement as a result of the construction works can be repaired at the applicants expense.

Community Involvement

62. The application is supported by a Statement of Community Involvement. The SCI provides a summary of the means by which the applicant and project team have communicated with and involved the following stakeholders:

- Brent Council Planning Service via the Major Cases Forum
- The local community – public exhibition held on 4 September 2015 and letters distributed to approximately 2000 residents in the area.
- Statutory organisations including Historic England and Twentieth Century Society.

63. The SCI also summarises the responses from the public exhibition. A total of 16 people attended, 8 of which provided feedback. The majority of the responses supported the proposal for a new swimming pool.

64. Your officers are satisfied that the pre-application public consultation, which is not a statutory duty, was carried out properly and without discrimination.

Conclusions

65. In conclusion, it is considered that the new swimming pool building at the front of the Lycee, will have limited degree of impact upon the significance of the Town Hall and its setting, and the surrounding area. It will enhance community facilities within the area, bringing both heritage and wider public benefits.

66. It is recommended that planning permission is granted, subject to the conditions as set out below.

SUPPLEMENTARY REPORT

Agenda Page Number: 57

Members Site Visit

Members visited the site on 12 December 2015. The following issues were raised:

Community Access Arrangements

The main committee report highlights that community access is proposed for the new swimming pool during the following times:

- The swimming pool will be open during the week for use by local schools for four half day sessions
- Community use of the swimming pool and studio facilities is anticipated to be before school use on weekdays, between 7 and 9am, and after school, between 7 and 9.30pm
- Community access for swimming lessons is anticipated on two early evening sessions per week, as well as weekend mornings.
- General community access to the pool facilities is expected on weekend afternoons and during holiday periods.

A community access plan is recommended to be secured as a condition to any forthcoming consent and the full community access details will be secured pursuant to this condition.

A 6-lane 25 m swimming pool is currently being delivered within the Dexion House site in Wembley. This is required to be publicly accessible which entry charges at local authority comparable rates.

Car park management

A car park management plan is to be conditioned to any forthcoming consent. This will include the requirement for the car park to be open for community access use during weekends. This has been secured following a request by officers in Transportation.

Relocation of bus stop and shelter

The applicant has been in discussions with Brent's Traffic Engineer and TfL regarding the relocation of the bus stop and shelter. The principle to relocate the bus stop and shelter has been agreed, but the exact location of the bus stop and shelter will need further consideration taking into account frequency of buses, the new pedestrian crossing on Forty Lane and street furniture/trees and the telephone booth on Forty Lane.

As discussed in condition 14, the applicant is required to make appropriate arrangements in writing to enter into an agreement with the Local Highway Authority and Transport for London for the works to the bus stop cage, shelter and associated works. Such works need to be approved by TfL and Brent's Transportation Unit prior to commencing works on the swimming pool building.

Pedestrian crossing improvements

The works regarding the pedestrian crossing on Forty Lane/The Paddocks are in progress, with the detailed design being considered by officers in Transportation. The traffic signals works (to be undertaken by TfL) cannot be commissioned until the pedestrian crossing works have been completed.

A variation of condition application was approved on 7 September 2015 (LPA Ref: 15/2790) to allow interim pedestrian access route and associated measures for a one year period in the event that the approved highway works were not fully implemented prior to first occupation of the school. The measures include the requirement for all entry into the site to be provided via the main entrance to the former Town Hall on the southern elevation of the building and to not use the access on The Paddocks.

Potential for vehicle damage at the junction with The Paddocks

The corner of the building has been set back away from the visibility splay at the junction of The Paddocks in response to concerns raised by Transportation. The amended layout has been reviewed by officers in Transportation and considered acceptable.

Memorial trees

The existing memorial trees are unable to remain in situ due to proximity to the swimming pool building. They will therefore need to be removed. Your officers have suggested that funding is secured for two new trees to be planted in Chalkhill Park with memorial leafs to be added to the Memorial Tree in the Civic Centre. An alternative option would be to have two new memorial trees planted in the lower landscape terrace of the site to the left hand side of the main steps into the building. If this is an option that members wished to pursue it is recommended that condition 9 is updated to add the following bullet point:

details of two replacement memorial trees (Flowering Cherry or similar species) to be planted in the lower landscape terrace on the left hand side of the main steps to the Lycee building

Impact of setting of listed building

As advised in the main committee report, the proposal has been subject to extensive pre-application discussions with Historic England and the 20th Century Society. Both of these statutory consultees have taken the view that the swimming pool building has been sensitively designed to respect the setting of the listed building, and read as a subservient addition to the main listed building.

The ancillary rooms (storage, plant, first aid and changing rooms) are to be situated under the car park. It is likely to be difficult to set the building further back from the road without reducing the width of the swimming pool itself as this would result in the open pool hall area projecting underneath the car park rather than just the ancillary rooms.

In light of the above, your officers are of the view that the proposed 1.4 m set back of the swimming building from the back edge of the pavement together with the height of the building at the same level as the car park is considered appropriate.

Full details of facing bricks for the swimming pool building together with details of windows (including depth of reveals) are recommended to be conditioned to any forthcoming consent.

Plant equipment and impact on neighbouring residential occupiers

The nearest residential property is on the opposite side of Forty Lane (No. 52 Forty Lane). A condition (No.

22) is recommended to be secured to any forthcoming consent requiring plant and ancillary equipment to be 10dB(A) below the measured background noise level when measured at the nearest noise sensitive premises.

Recommendation: Remains approval

CIL DETAILS

This application is liable to pay **£0.00*** under the Community Infrastructure Levy (CIL).

We calculated this figure from the following information:

Total amount of eligible** floorspace which on completion is to be demolished (E): sq. m.

Total amount of floorspace on completion (G): 13774 sq. m.

Use	Floorspace on completion (Gr)	Eligible* retained floorspace (Kr)	Net area chargeable at rate R (A)	Rate R: Brent multiplier used	Rate R: Mayoral multiplier used	Brent sub-total	Mayoral sub-total
Non-residential institutions	13774	12580	1194	£0.00	£0.00	£0.00	£0.00

BCIS figure for year in which the charging schedule took effect (Ic)	224	224
BCIS figure for year in which the planning permission was granted (Ip)	259	
Total chargeable amount	£0.00	£0.00

*All figures are calculated using the formula under Regulation 40(6) and all figures are subject to index linking as per Regulation 40(5). The index linking will be reviewed when a Demand Notice is issued.

****Eligible** means the building contains a part that has been in lawful use for a continuous period of at least six months within the period of three years ending on the day planning permission first permits the chargeable development.

Please Note : CIL liability is calculated at the time at which planning permission first permits development. As such, the CIL liability specified within this report is based on current levels of indexation and is provided for indicative purposes only. It also does not take account of development that may benefit from relief, such as Affordable Housing.

DRAFT DECISION NOTICE



Brent

DRAFT NOTICE

TOWN AND COUNTRY PLANNING ACT 1990 (as amended)

DECISION NOTICE – APPROVAL

=====

Application No: 15/4140

To: Miss Hill
Nathaniel Lichfield & Partners
14 Regent's Wharf
All Saints Street
London
N1 9RL

I refer to your application dated 24/09/2015 proposing the following:
Erection of a part basement building fronting Forty Lane to house a five lane swimming pool and studio with a green roof and associated works to include courtyard entrance, security gates, cycle parking, demolition and reinstatement of retaining wall, landscaping and installation of PV panels on the roof of the new Annex building
and accompanied by plans or documents listed here:
Refer to Condition 2
at Lycee International De Londres, 54 Forty Lane, Wembley, HA9 9LY

The Council of the London Borough of Brent, the Local Planning Authority, hereby GRANT permission for the reasons and subject to the conditions set out on the attached Schedule B.

Date:

Signature:

Head of Planning, Planning and Regeneration

Notes

1. Your attention is drawn to Schedule A of this notice which sets out the rights of applicants who are aggrieved by the decisions of the Local Planning Authority.
2. This decision does not purport to convey any approval or consent which may be required under the Building Regulations or under any enactment other than the Town and Country Planning Act 1990.

DnStdG

SUMMARY OF REASONS FOR APPROVAL

- 1 The proposed development is in general accordance with policies contained in the:-

The National Planning Policy Framework (NPPF)
 The London Plan - The Spatial Development Strategy for London Consolidated with Alterations since 2011 (adopted March 2015)
 Brent Unitary Development Plan 2004
 Brent Core Strategy 2010
 Wembley Area Action Plan

Relevant policies in the Adopted Unitary Development Plan are those in the following chapters:-

Built Environment: in terms of the protection and enhancement of the environment
 Environmental Protection: in terms of protecting specific features of the environment and protecting the public
 Employment: in terms of maintaining and sustaining a range of employment opportunities
 Transport: in terms of sustainability, safety and servicing needs
 Community Facilities: in terms of meeting the demand for community services
 Wembley Regeneration Area: to promote the opportunities and benefits within Wembley

- 1 The development to which this permission relates must be begun not later than the expiration of three years beginning on the date of this permission.

Reason: To conform with the requirements of Section 91 of the Town and Country Planning Act 1990.

- 2 The development hereby permitted shall be carried out in accordance with the following approved drawing(s) and/or document(s):

WFS LSI A31 SWI GA SL 000100 Rev A - Location Plan
 WFS LSI A31 SWI GA SL 000101 Rev A - Existing Site Plan
 WFS LSI A31 SWI ELV SL 000102 Rev A - Existing Elevations and Sections
 WFS LSI A31 SWI GA GND 000200 Rev B - Proposed Ground Floor Plan
 WFS LSI A31 SWI GA RF 000201 Rev B - Proposed Roof Plan
 WFS LSI A31 SWI ELV SL 000202 Rev B - Proposed Elevations
 WFS LSI A31 SWI SEC SL 000203 Rev B - Proposed Sections
 WFS LSI A31 SWI CGI SL 000204 Rev A - Proposed 3D Visual
 WFS LSI A31 SWI EXT SL 000300 Rev B - Proposed Landscape Scheme
 WFS LSI A31 SWI DET SL 000400 Rev A - Proposed Parapet Detail
 WFS LSI A31 SWI DET SL 000401 Rev A - Indicative Isometric View
 WFS LSI A31 SWI DET SL 000402 Rev A - Proposed Window & Door Details
 WFS LSI A31 SWI DET SL 000403 Rev A - Proposed Courtyard Security Gate
 WFS LSI A31 SWI DET SL 000404 Rev A - Proposed Plant Equipment Details
 WFS LSI A31 SWI GA SL 000500 Rev A - Proposed Demolition Plan

Supporting Documents

Archaeological Desk-Based Assessment prepared by Wessex Archaeology dated September 2013 (Ref: T17401.01)
 Arboricultural Report - prepared by CBA Trees dated September 2015 (Ref: CBA10161 v 1B Pool)
 Coach Management Plan - prepared by TTP Consulting dated September 2015
 Community Access Plan - prepared by Bouygues UK Ltd dated 19 August 2015 (Ref: WSP BYG A0 ALL REP SL 000001 Rev A)
 Design and Access Statement - prepared by LSI dated September 2015

Energy Statement - prepared by Harley Haddox dated September 2015 (amended October 2015 Issue 05)
 External Lighting Assessment Rev 02 - prepared by Harley Haddox
 External Noise Assessment - prepared by Aecom dated August 2015 (Ref: 60300819)
 Flood Risk Assessment - prepared by Ramboll Environ dated 22 September 2015 (Ref: UK14-18934)
 Foul Drainage and Utilities Statement - prepared by Bouygues UK Ltd dated 24 August 2015 (Ref: WSP BYG A0 ALL REP SL 000001 Rev A)
 Heritage Impact Assessment - prepared by NLP dated 18 September 2015 (Ref: 13349/NG/KD)
 Planning Statement - prepared by NLP dated 24 September 2015 (Ref: 13349/NG/RHi)
 Preliminary Ecological Assessment (Habitat Survey) - prepared by Middlemarch Environmental dated October 2015 (Ref: RT-MME-120792-01)
 Interim School Travel Plan - prepared by TTP Consulting dated September 2015
 Site Waste Management Plan - prepared by Bouygues UK Ltd dated 19 August 2015 (Ref: WSP BYG A0 ALL REP SL 000001 Rev A)
 Statement of Community Involvement - prepared by NLP dated 24 September 2015 (Ref: 13349/NG/JHy)
 Sustainability Statement and Checklist - prepared by Bouygues UK Ltd dated September 2015
 Transport Statement - prepared by TTP Consulting dated September 2015
 Ventilation Statement Rev 01 - prepared by Harley Haddox

Reason: For the avoidance of doubt and in the interests of proper planning.

- 3 No music, public address system or any other amplified sound system shall be installed or used externally on the site without the prior written approval of the Local Planning Authority. Any proposed system(s) shall be submitted to and approved in writing by the Local Planning Authority prior to installation and thereafter only installed and operated in accordance with the details so approved.

Reason: To safeguard the amenities of the adjoining occupiers.

- 4 Vegetation clearance shall be undertaken outside of the nesting bird season (generally extends between March and September inclusive). If this is not possible then any vegetation that is to be removed or disturbed shall be checked by an experienced ecologist for nesting birds immediately prior to works commencing. If birds are found to be nesting any works which may affect them is required to be delayed until the young have fledged and the nest has been abandoned naturally.

Reason: To ensure compliance with the Wildlife and Countryside Act 1981 (as amended).

- 5 The swimming pool building hereby approved shall be ancillary to the use of the main school building within the site (with the exception of community access as secured as part of this planning consent), unless otherwise agreed in writing by the Local Planning Authority.

Reason: To enable to Local Planning Authority to consider the highway impact of in dependant use of the swimming pool building.

- 6 No development shall take place, including any works of demolition, until a Construction Method Statement and Construction Logistics Plan has been submitted to, and approved in writing by, the local planning authority. The approved Statement shall be adhered to throughout the construction period. The Statement shall provide for:

(i) The best practical means available in accordance with British Standard Code of Practice BS5228-1:2009 shall be employed at all times to minimise the emission of noise from the site;

(ii) The operation of the site equipment generating noise and other nuisance causing activities, audible at the site boundaries or in nearby residential properties shall only be carried out between the hours of 08:00 - 18:00 Mondays-Fridays, 08:00 -13:00 Saturdays and at no time on Sundays or Bank Holidays unless otherwise agreed in writing by the Local Planning Authority;

(iii) Details on how the bus stop and pavement will not be blocked or impeded from its

normal usage during construction;

- (iv) A photographic dilapidation survey of the pavement;
- (v) the parking of vehicles of site operatives and visitors;
- (vi) construction traffic routes to the development site;
- (vii) loading and unloading of plant and materials;
- (viii) storage of plant and materials used in constructing the development;
- (ix) the erection and maintenance of security hoarding including decorative displays and facilities for public viewing, where appropriate;
- (v) wheel washing facilities and schedule of highway cleaning;
- (xi) measures to control the emission of dust and dirt during construction;
- (xii) a scheme for recycling/disposing of waste resulting from demolition and construction works;
- (xiii) School access during the construction phase (including staff car parking, servicing and delivery arrangements);
- (xiii) Enter into the Considerate Contractors Scheme

Reason: To protect residential amenity and ensure the development does not have an adverse impact on the highway.

- 7 No works shall commence on site prior to a Tree Protection Plan and Arboricultural Method Statement being submitted to and approved in writing by the Local Planning Authority. These shall include method statements and plans which:

- (i) adhere to the principles embodied in BS5837:2012
- (ii) indicate exactly how and when the retained trees on-site or off-site near the site boundaries will be protected during the construction works;
- (iii) show root-protection zones;
- (iv) detail method of pruning works to retained trees

Provision shall also be made for supervision of tree protection by a suitably qualified and experience arboricultural consultant and details shall be included within the tree protection statement. No works shall commence on site until the Council's Tree Officer has been on site to inspect the protection measures and, the development shall be carried out strictly in accordance with the agreed details.

Reason: To ensure retention and protection of trees on the site in the interests of amenity.

- 8 Prior to commencement of building works on site, further details of the following external materials (with samples to be pre-arranged to be viewed on site) shall be submitted to and approved in writing by the Local Planning Authority. Such details shall include:

- (i) Details of facing bricks including a sample board showing details of the mortar
- (ii) Details of the specification and colour of the window frames and doors
- (iii) Details of the finishing material and colour for parapet roof

The works shall be carried out in full accordance with the approved details, unless alternative materials are agreed in writing by the Local Planning Authority.

Reason: To ensure a satisfactory development which does not prejudice the setting of the listed building.

- 9 Prior to commencement of building works on site, further details of the following shall be submitted to and approved in writing by the local planning authority. Such details shall include drawings, including sections where appropriate, at a suitably large scale (e.g. 1:5, 1:10, 1:20, 1:50 as appropriate) or manufacturer's literature which show:

- (a) the window and door reveals, headers and sills, including the depth of the reveals and the junction of materials around the returns;
- (b) measures to screen the open roof of the plant room;
- (c) details of the design, material and height of the external flue

- (d) details of the design and materials of the security gates

The development shall be completed in accordance with the details so approved before the swimming pool building is occupied.

Reason: These details are required to ensure that a satisfactory development is achieved.

- 10 Notwithstanding the plans hereby approved, within six months of commencing of works on site, further details of all hard and soft landscape works within the site shall be submitted to and approved in writing by the Local Planning Authority. The hard and soft landscaping shall be completed prior to first occupation of the swimming pool building hereby approved.

Details shall include:

- (a) all planting (including location, species, size, density) of replacement trees within the car park and along The Paddocks frontage as shown in the Proposed Landscape Scheme, details of low level planting along the front of the building, and planting within the courtyard;
- (b) specification of the green roof over the swimming pool building to include section plans and construction methodology, full details of all planting within the green roof (including location, species, size, density) that incorporating a biodiversity enhancement scheme as suggested within the Ecology Statement;
- (c) Details of bat boxes (suitable for pipistrelle species) and bird boxes (suitable for house sparrow) and
- (d) Details of ramped access for terrestrial mammals if excavations are left open overnight during the construction stages
- (e) Details of hard surface materials within the site including means of demarcated the private forecourt from the public highway
- (f) Details of external lighting including light spillage diagrams

Any trees and shrubs planted in accordance with the landscaping scheme which, within 5 years of planting, are removed, dying, seriously damaged or become diseased, shall be replaced in similar positions by trees and shrubs of similar species and size to those originally planted unless otherwise agreed in writing with the Local Planning Authority.

Reason: To ensure a satisfactory standard of appearance and setting for the development and to ensure that the proposed development enhances the visual amenity of the locality in the interests of the amenities of the occupants of the development and to provide tree planting in pursuance of section 197 of the Town and Country Planning Act 1990.

- 11 Prior to commencement of works on site, further details of the retaining walls as set out in the Proposed Demolition Plan shall be submitted to and approved in writing by the Local Planning Authority. Such details shall include:

- (a) details of the methodology for protecting the existing retaining wall along the stepped access to the site from Forty Lane during construction works;
- (b) details of the methodology for the demolition of the retaining wall along the car park and the low boundary wall along the frontage;
- (c) details of the methodology for the rebuilding of the retaining wall along the car park (with the additional five courses) and the low boundary wall along the frontage, including details of materials and mortar (to be prearranged to be viewed on site) and elevation plans.

The approved methodologies shall be carried out in full accordance with the approved details, and the swimming pool building shall not be occupied until the retaining wall along the car park and the low boundary wall along the frontage have been rebuilt to the satisfaction of the Local Planning Authority.

Reason: In the interests of the setting of the listed building.

- 12 Prior to commencement of the development, the applicant shall make appropriate arrangements in writing to enter into an agreement with the Local Highway Authority and Transport for London to provide the following highway works:

- (i) works to the bus stop cage, shelter and associated works.

The new swimming pool building shall not be occupied (or other timescales to be agreed in writing by the Local Planning Authority) until the above works have been completed to the satisfaction of the Local Highway Authority and have been certified in writing as being substantially complete by or on behalf of the local planning authority. The works shall be carried out at the applicants expense.

Reason: In the interests of highway and pedestrian safety.

- 13 Within 2 months of occupation of the new swimming pool building, a Energy Assessment Review shall be submitted to and approved in writing by the Local Planning Authority. This review by an approved independent body shall verify that the development has met or exceeded the following:

- (i) Minimum 35% improvement on Part L 2013 Building Regulations Target Emission Rate ("TER") for CO2 emissions;
- (ii) BREEAM 'Excellent' rating
- (iv) Minimum score of 44% on Brent's Sustainability Checklist

If the review specifies that the development has failed to meet the above levels, compensatory measure shall be submitted to and approved in writing by the Local Planning Authority prior to the occupation of the development.

The approved Sustainability and Energy Strategies (or as amended) shall be fully implemented and maintained for the lifetime of the Development unless otherwise agreed in writing with the Council.

Reason: To ensure a satisfactory development which incorporates sustainability measures that are commensurate to the scale of development proposed.

- 14 Prior to the commencement of the use of the new swimming pool buildings, an updated School Travel Plan of sufficient quality to score a PASS rating when assessed under Transport for London's ATTrBuTE programme (or any replacement thereof), to incorporate targets for minimising car use, monitoring of those targets and associated measures to meet those targets, shall be submitted to and approved in writing by the Local Planning Authority.

On first Occupation of the new school building the Travel Plan shall be fully implement for the lifetime of the Development, or as amended by the agreement of the Local Planning Authority in writing.

Reason: In order to promote sustainable transport measures where on-street parking and manoeuvring may cause highway safety problems.

- 15 Prior to first occupation of the swimming pool building hereby approved, further details of an access strategy for the building shall be submitted to and approved in writing by the Local Planning Authority. This shall include details of how inclusive access is provided for all users. The approved access strategy shall thereafter be implemented in full when the building is first occupied, and thereafter retained throughout the lifetime of the development.

Reason: In the interests of inclusive access.

- 16 Prior to occupation of the new swimming pool building, a car park management plan shall be submitted to and approved in writing by the Local Planning Authority. This shall include the management of car parking on site for community access during weekends. The car park management plan shall be implemented in accordance with the approved details, and maintained throughout the lifetime of the development unless otherwise agreed in writing with the Local Planning Authority.

Reason: In the interest of highway safety.

- 17 Prior to occupation of the new swimming pool building, a servicing and delivery plan for deliveries, refuse collection and coaches shall be submitted to and approved in writing by the Local Planning Authority, and thereafter implemented in accordance with the approved details throughout the lifetime of the development, unless otherwise agreed in writing by the Local Planning Authority.

Reason: In the interest of highway safety and the amenities of neighbouring occupiers.

- 18 Prior to the occupation of the new swimming pool building, a Community Access Plan shall be submitted to and approved in writing by the Local Planning Authority, and a copy of the approved agreement has been provided to the Local Planning Authority. The Plan shall allow for a minimum of 30 hours of community use each week. The Plan shall include details of:

- (i) written notification to the local planning authority of the date of implementation of the Plan within 21 days of commencement;
- (ii) the facilities to be made available including: the swimming pool, activity studios and ancillary changing rooms and parking facilities
- (iii) access by non-school users/non-members and management responsibilities including the ongoing promotion of the availability of the facilities for community use including up-to-date information about this on the school website;
- (iv) rates of hire based upon and comparable with those charged at other public facilities;
- (v) terms of access;
- (vi) a mechanism for review after one year following implementation of the Plan

The approved Community Access Plan shall be brought into operation within 3 months of occupation of the new school and it shall remain in operation for the duration of the use of the development.

Reason: To secure well-managed, safe community access to the sports and other community facilities, to ensure sufficient benefit to the development of sport and to accord with local and regional policies for the maximum use of community facilities.

- 19 Prior to the installation of any plant equipment, an assessment of the noise level from any installed plant (such as air handling units, generators, ventilation/extraction systems) together with any associated ducting, achieving 10 dB(A) or greater below the measured background noise level at the nearest noise sensitive premises, shall be submitted to and approved in writing by the Local Planning Authority. The method of assessment shall be carried out in accordance with BS4142:1997 'Rating industrial noise affecting mixed residential and industrial areas'. If the predicted noise levels of the plant exceed those specified within this condition, then a scheme of insulation works to mitigate the noise shall be included. The recommendations provided within the noise assessment together with any required mitigation measures shall be carried out in full accordance with the approved details prior to first use of the plant equipment hereby approved.

Reason: To protect that users of the surrounding area do not suffer a loss of amenity by reason of noise nuisance.

- 20 Prior to first occupation of the swimming pool building hereby approved, details shall be submitted to and approved in writing by the Local Planning Authority of the boilers and/or CHP system installed, demonstrating that the rated emissions of Oxides of Nitrogen (NOx) do not have an adverse impact on local air quality. If any mitigation measures are required, these shall be carried out in full accordance with the approved details prior to first use of the boilers and/or CHP system hereby approved.

Reason: To protect local air quality.

- 21 Development shall not commence until a drainage strategy detailing any on and/or off site drainage works, has been submitted to and approved by, the local planning authority in consultation with the sewerage undertaker. No discharge of foul or surface water from the site shall be accepted into the public system until the drainage works referred to in the strategy have

been completed.

Reason: The development may lead to sewage flooding; to ensure that sufficient capacity is made available to cope with the new development; and in order to avoid adverse environmental impact upon the community.

- 22 Prior to first occupation of the new swimming pool building, further details of the PV panels on the roof of the annex building (including the size, location and orientation of the photovoltaic panels) in accordance with the sustainability measures secured as part of this development, together with details of how the PV panels will be linked to the swimming pool building, shall be submitted to and approved in writing by the Local Planning Authority. The PV panels shall be installed prior to first occupation of the swimming pool, and thereafter maintained throughout the lifetime of the development unless otherwise agreed in writing by the Local Planning Authority.

Reason: In the interests of sustainability and the setting of the listed building.

- 23 Prior to occupation of the new school, further details of the cycle spaces within the courtyard with details of how the cycle stands will be secure, shall be submitted to and approved in writing by the local planning authority. The works shall be carried out in accordance with the approved details and retained thereafter for the lifetime of the development.

Reason: To ensure satisfactory level of cycle parking.

- 24 Prior to first occupation of the swimming pool building hereby approved, further details of external signage (to be conservation style) including details of any illumination shall be submitted to and approved in writing by the Local Planning Authority. The signage shall thereafter be installed in accordance with the approved details.

Reason: In the interests of the setting of the listed building and highway safety.

- 25 (a) Prior to the commencement of works, a Training & Employment Plan shall be submitted to and approved in writing by the Local Planning Authority which shall include but not be limited to the following (unless otherwise agreed in writing by the Local Planning Authority):

1. Details of the Training & Employment Co-ordinator;
2. A methodology to target:
 - i. 1 in 10 of the projected number of construction jobs being held by Brent Residents and
 - ii. the provision of training for a previously unemployed Brent Resident or School leaver for 1 in 100 construction jobs;
3. Details of the procedures to report the provision of jobs and training to the Council's Head of Employment;
4. A commitment to offer an interview to any job applicant who is a resident in Brent provided that they meet the minimum criteria for the particular job;

The approved Training and Employment Plan shall be implemented throughout the construction phases of the development for the lifetime of the construction of the Development.

(b) Prior to occupation of the building, a Training & Employment Verification Report shall be submitted to and approved in writing by the Council. The report shall set out how the measures approved pursuant to part (a) of this condition have been implemented and shall evidence the number of jobs and training opportunities held by Brent Residents during construction.

Reason: In the interest of providing local employment opportunities.

Any person wishing to inspect the above papers should contact Victoria McDonagh, Planning and Regeneration, Brent Civic Centre, Engineers Way, Wembley, HA9 0FJ, Tel. No. 020 8937 5337